

Mr. PRISK. That is right.

Mr. CONSTANDY. When you make reference to the safety devices, are you speaking of the guardrail?

Mr. WILSON. Guardrail, delineation. I do not think I see a stripe—oh, yes, there is a stripe there.

Mr. CONSTANDY. I think it is interesting, is it not, that Missouri, Mr. Prisk, has a different philosophy about the delineators? Or do I have the wrong State?

Mr. PRISK. Excuse me, I did not quite get the question.

Mr. CONSTANDY. The delineators you normally find on the edge of the shoulder on the highways, do they have a different philosophy?

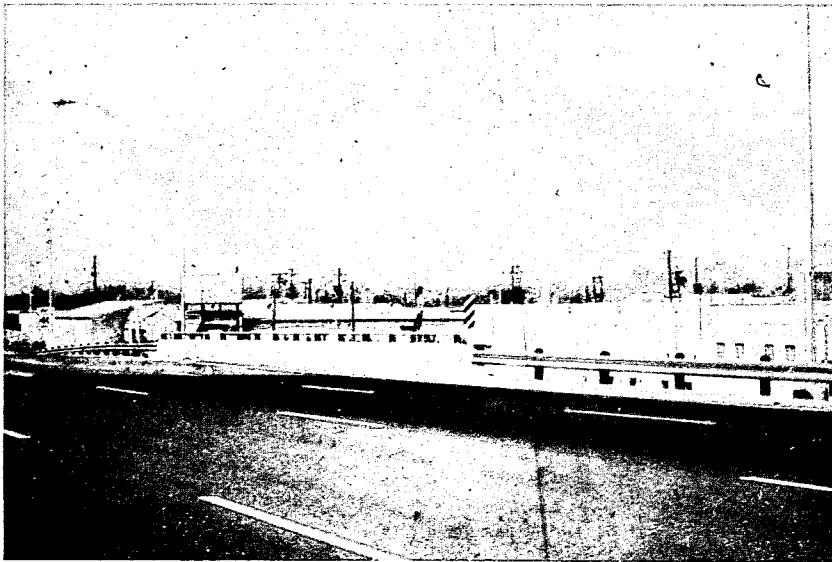
Mr. PRISK. Yes. As I recall, during the conversations on this project, we were advised that the State of Missouri did not want to use delineators on the Interstate project.

Mr. CONSTANDY. This is an optional thing, whether you do it or not?

Mr. PRISK. It is not an optional thing; it is required.

Mr. CONSTANDY. Maybe it lends more significance to the fact that while they do not believe in it, generally, they do put the three amber ones on the end of the bridge?

Mr. PRISK. That is right.



Here now we come to Oklahoma, and the project that we see there, again, presents the same difficulty essentially with a bridge in an urban area where there is no transition treatment.

There has been an attempt here at a transition treatment, where a portion of that wall has been recessed to anchor a bolt and to permit a single five-eighths-inch bolt to go through the end guardrail into the concrete, so as to sustain that last panel.