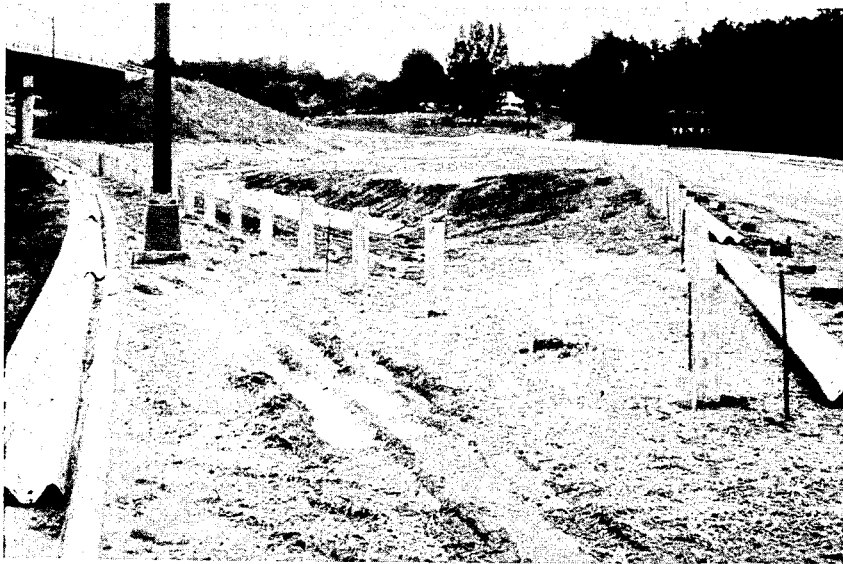


Here is another part of the same new work showing, rather interestingly, a deliberate placement of rail rather well back from the nose or gore decision point, which is about where the camera is, but interestingly enough leaving the light pole in here in an exposed position in relation to this rail that is going to be built around through here.



Mr. ZION. That is apparently not a breakaway type of pole either.

Mr. PRISK. No.

Mr. ZION. Is that a concrete foundation it is sitting on?

Mr. PRISK. Yes, sir, it is a concrete foundation.

Mr. ZION. Does this not cost more than the aluminum breakaway-type poles you were showing previously?

Mr. PRISK. They are competitive in most cases.

Mr. ZION. Again would this not indicate a need for establishing some Federal criteria for light poles as well as for placement of guardrails, and tying them to bridges in highway construction?

Mr. PRISK. I think that it would deserve commensurate consideration along with these other items that you mention as far as the performance standards are concerned, yes.

I do not think that you need to limit materials to aluminum or steel. You can accomplish performance with either material. I do not condemn one or the other.

Mr. ZION. Certainly we are not fighting the concrete people, but certainly this is one of the danger elements that could be reduced in erecting light poles; do you not agree?

Mr. PRISK. If you are talking about that base, I fully agree, yes.

Mr. CONSTANDY. Congressman, you raise an interesting point, if you will allow a comment about the need for Federal standards to regulate some of this.